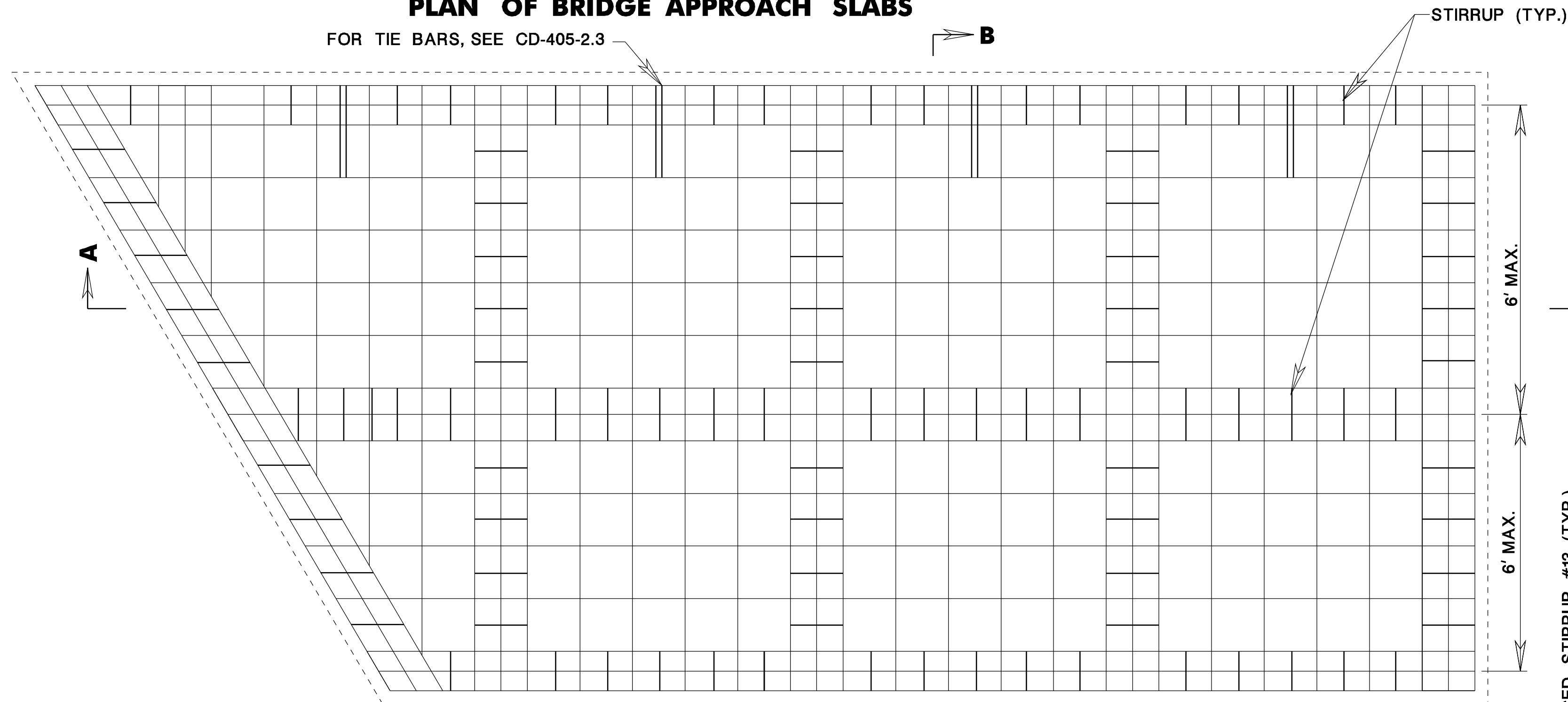
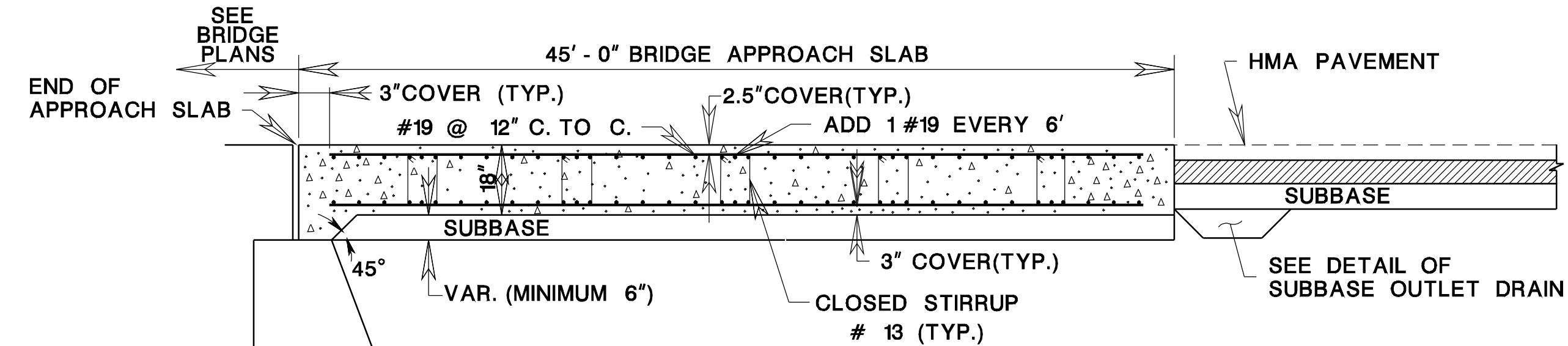
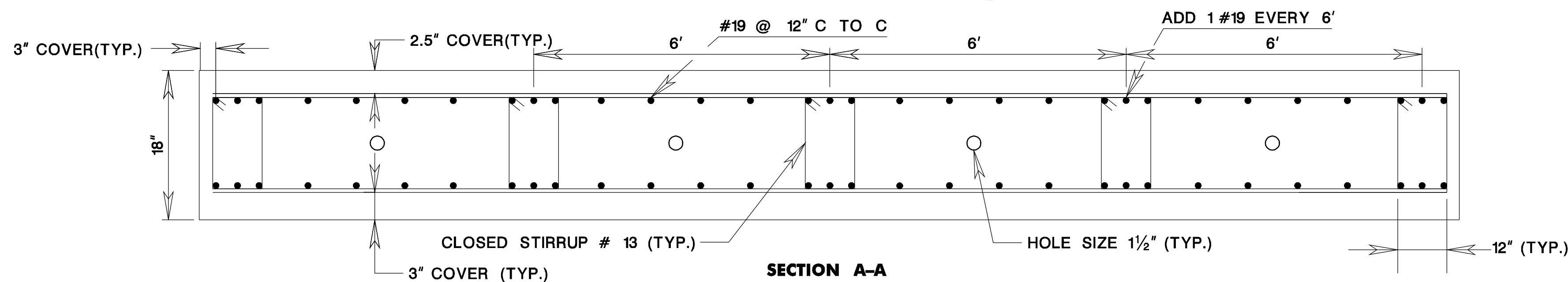


PLAN OF BRIDGE APPROACH SLABS

FOR TIE BARS, SEE CD-405-2.3

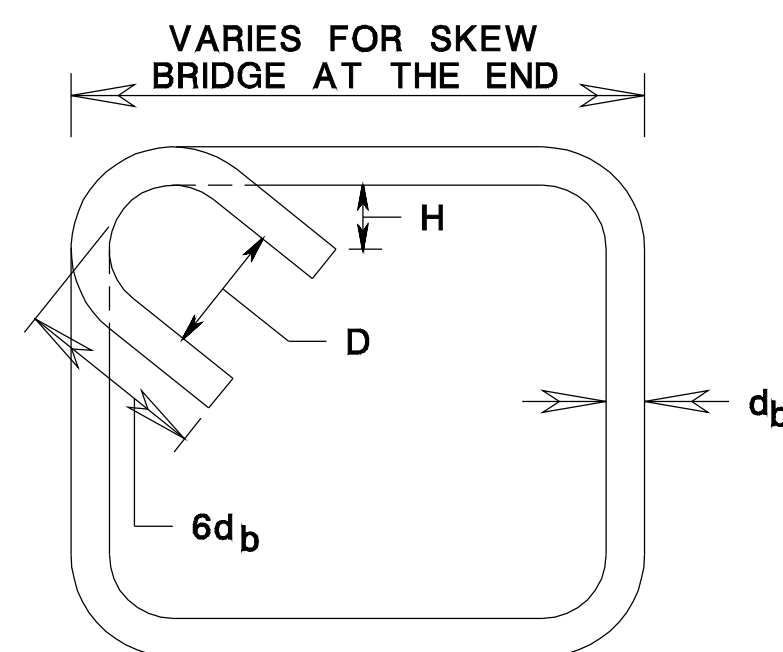


SLAB SECTION

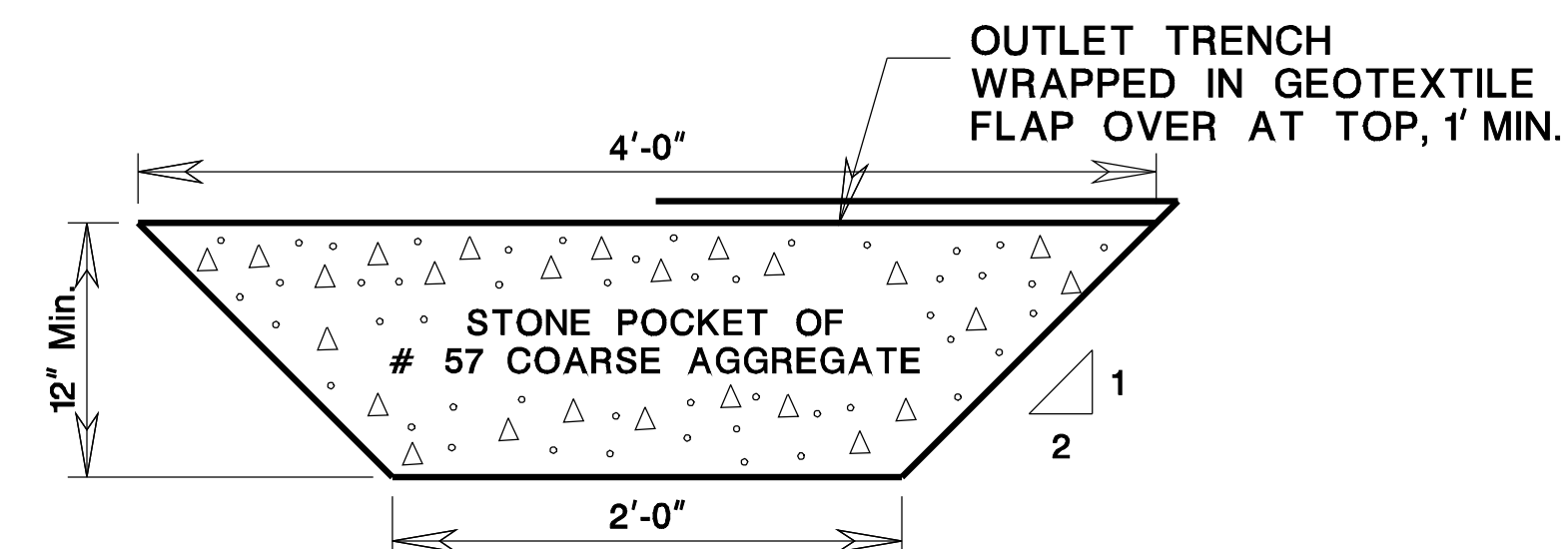


NOTE :
ALL LONGITUDINAL AND TRANSVERSE REINFORCEMENT STEEL TO BE SECURELY WIRED TOGETHER.

SECTION OF BRIDGE APPROACH SLABS



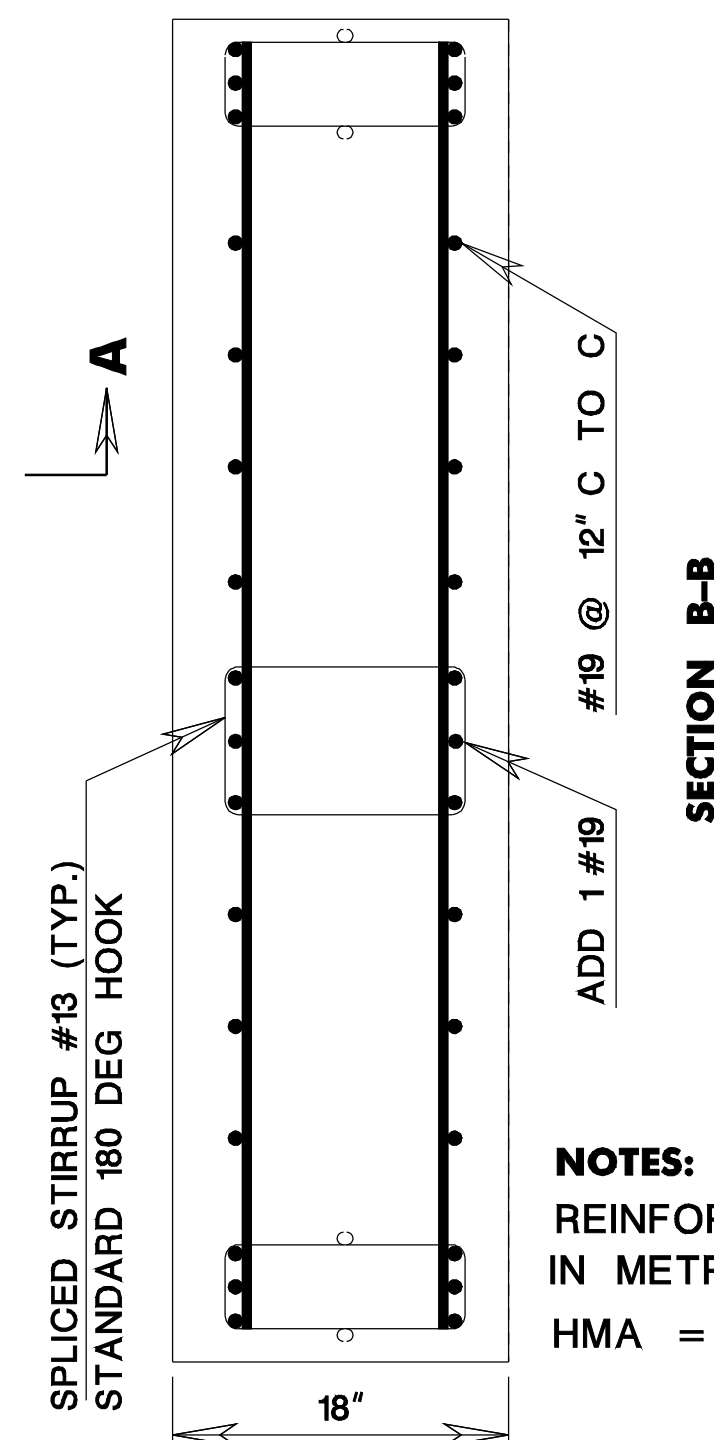
CLOSED STIRRUP # 13 (TYP.)



SUBBASE OUTLET DRAIN

NOTES:

- EXTRA REINFORCEMENT STEEL FOR EMBEDDED BEAM (EB) PATTERN TO BE SPACED NOT MORE THAN 6 FEET ON CENTERS.
- INSTALL REINFORCEMENT STEEL CHAIRS TO SEPARATE TOP AND BOTTOM MATS (AS SHOWN).
- WIDER/LONGER SLABS MAY BE CONSTRUCTED PROVIDED THE C TO C SPACING OF EB EXTRA REINFORCEMENT STEEL IS NOT MORE THAN 6 FEET.
- ALTERNATE DESIGN WITH STIRRUPS: CLOSED STIRRUPS SUBSTITUTED FOR REINFORCEMENT STEEL CHAIRS. SPACING NOT MORE THAN 3 FEET C TO C. STIRRUP MUST ENCLOSE TOP 3 AND BOTTOM 3 BARS.
- ALL REINFORCEMENT STEEL TO BE CORROSION PROTECTED.
- CONSTRUCT APPROACH SLABS WITH CLASS A CONCRETE. BASIS OF PAYMENT SHALL BE CUBIC YARDS. THE DEPARTMENT WILL MAKE PAYMENT FOR REINFORCEMENT STEEL UNDER REINFORCEMENT STEEL, EPOXY COATED OR REINFORCEMENT STEEL, GALVANIZED. APPROPRIATE PAY ITEM AS SPECIFIED IN THE STANDARD SPECIFICATION SHOULD BE USED.
- THE APPROACH SLAB SHALL BE SAWCUT GROOVED FINISHED. THE METHODS USED SHALL BE ACCORDING TO THE REQUIREMENTS SPECIFIED IN THE NJDOT STANDARD SPECIFICATIONS FOR CONCRETE DECK SLABS.
- BENDS IN STIRRUPS SHALL CONFORM TO CRSI STANDARDS.
- REFER TO BCD-507-6 FOR EXPANSION JOINT DETAILS.

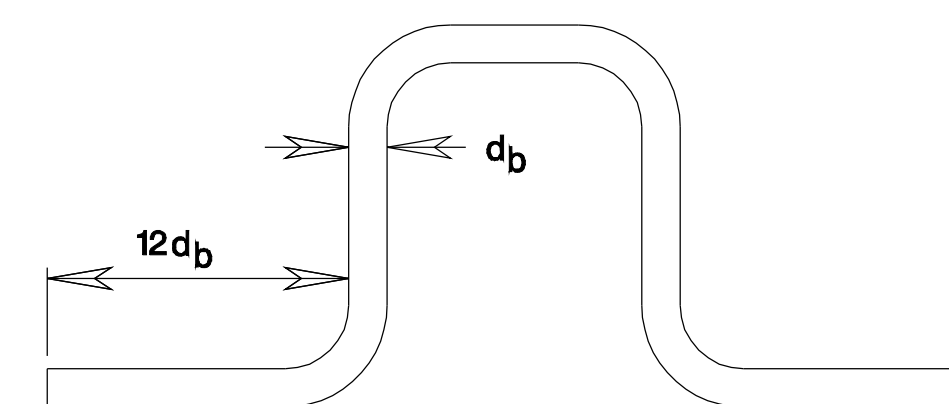


NOTES:

REINFORCEMENT STEEL IS DESIGNATED IN METRIC UNITS.
HMA = HOT MIX ASPHALT

BRIDGE APPROACH SLABS ADJOINING HMA PAVEMENT

N.T.S.



ALTERNATE # 13 STIRRUP SPLICED IN PAIRS

BCD-507-5.1