

RGGI Medium and Heavy-Duty Charging Program

May 29, 2026

Public Stakeholder Session

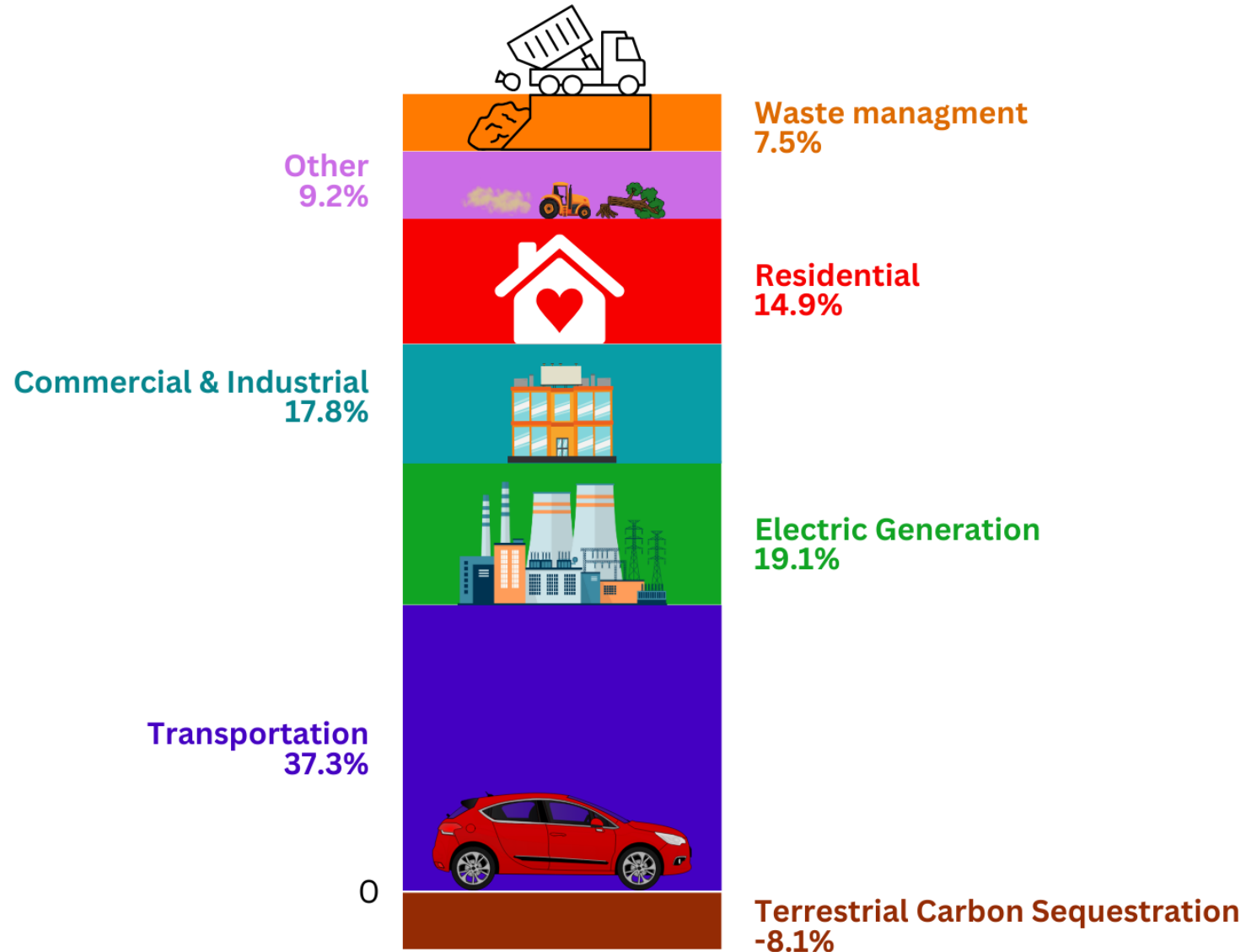
Aviv Bernstein Livne
Transportation Coordinator



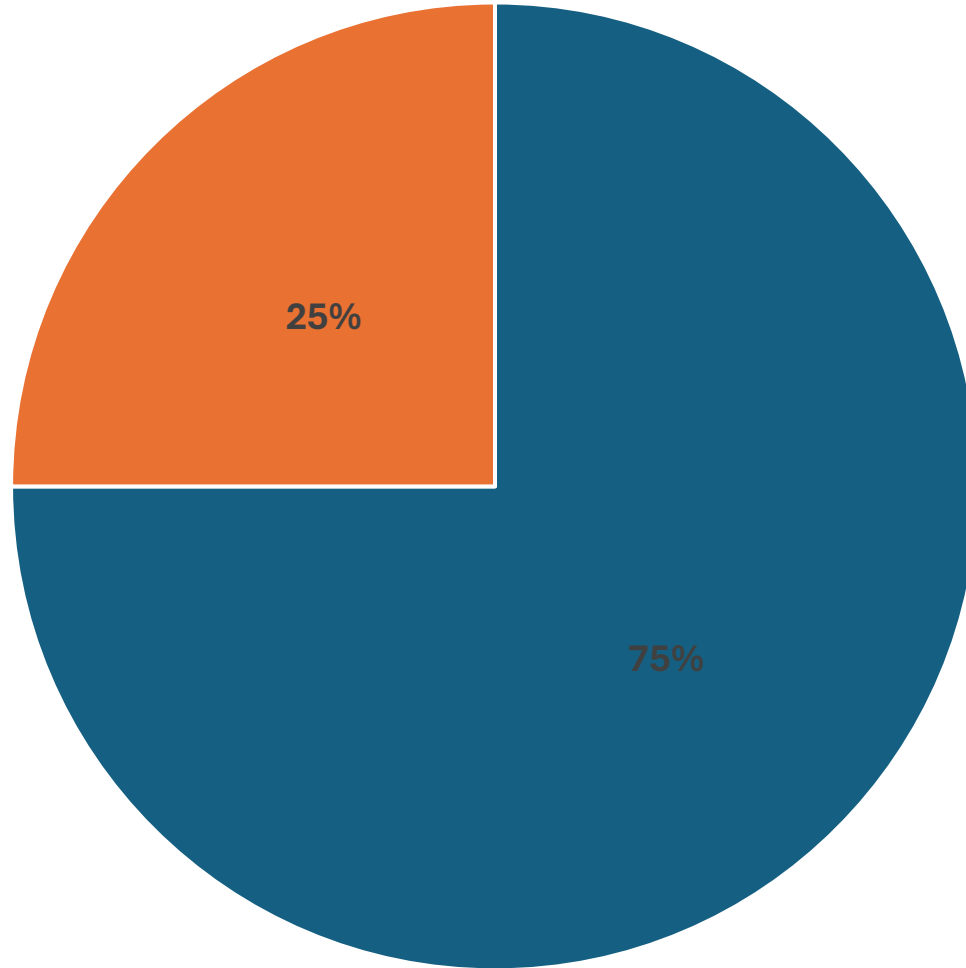
Background



New Jersey Emissions by Sector



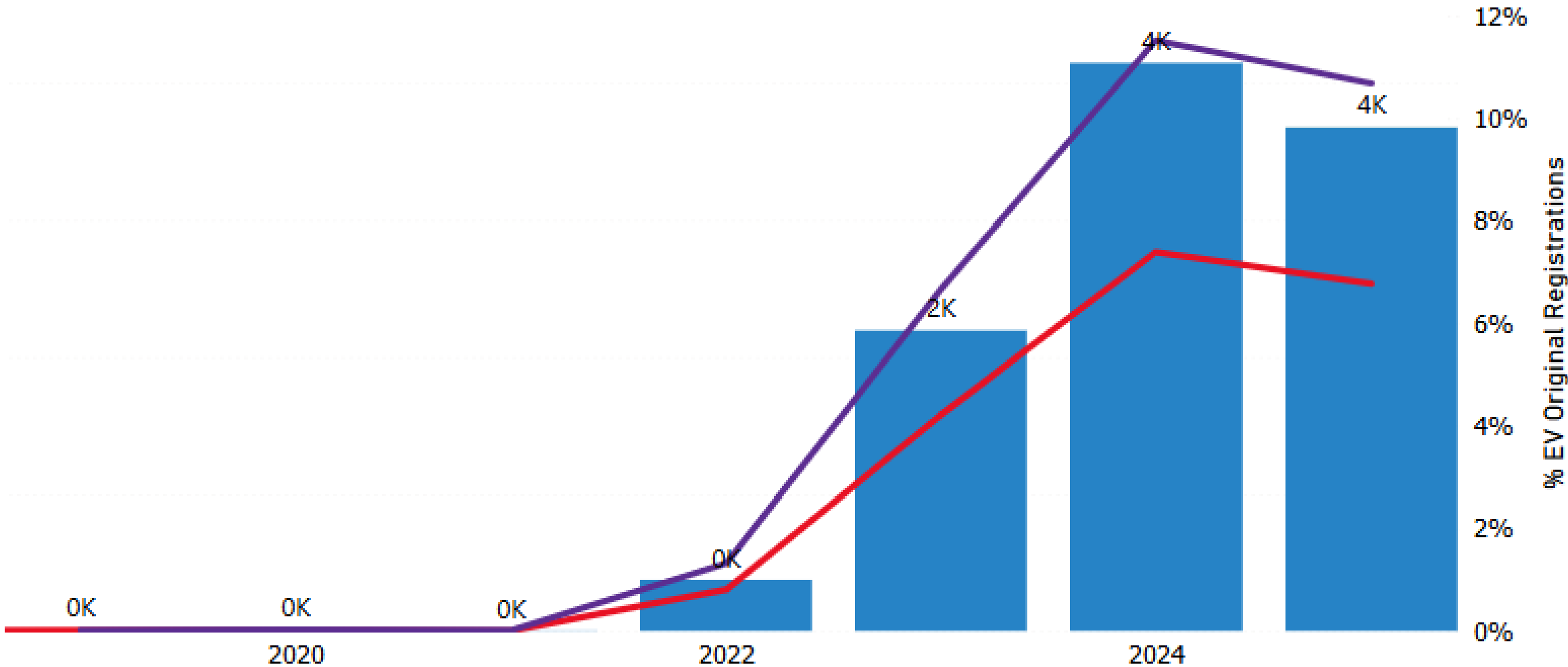
Transportation Emissions



■ Light Duty ■ Medium & Heavy Duty

Original EV Registrations

Original Registrations ● EV Share of Original Registrations ● EV Share of Original Registrations (Most Recent Model Years Only)



Challenges

- Upfront cost of equipment and charging.
- Extremely limited access to public charging.

First round funded 24 ports at the State's first publicly accessible MHD depot!

A second will imminently be coming online.

The screenshot shows a map application interface. At the top, there are three tabs: "Public Stations" (selected), "Advanced Filters", and "Fuel Corridors". The map displays the New York City metropolitan area, including parts of New Jersey and Staten Island. A green dot is placed on the map near Newark, indicating a station location. On the right side, a dark grey panel displays the search results: "1 station location" and "30 EV charging ports". Below this, it shows "Filters chosen:" with three categories: "New Jersey" (with a map icon), "Electric" (with a water drop icon) and "Types: DC Fast", and "Access: Public" (with a gas pump icon) and "Vehicle Accessibility: Medium-duty (class 3-6)". At the bottom of the panel, there are two download options: "Download Station Locations" and "Download EV Charging Ports".

Program



Program Overview

- The goal is to help businesses gain access to MHD charging, improve air quality, and help the state meet its climate goals.
- Approximately \$10 million in available funding
- Awards are \$225,000 per DCFC, up to 6 DCFC per application
- A single applicant can submit multiple applications if the applicant is applying for multiple locations
- Eligible applicants are businesses in Overburdened Communities (“OBC”)
- **Application deadline 7/31/2026 at 5:00 p.m. via SAGE**

Awards

- \$225,000 per DCFC
 - Eligibility: Located or operating in overburdened communities.
 - Ideal locations: Accessible by several local businesses that have, or are interested in acquiring, MHD EVs, such as warehouses or depots.
 - Must be public or Semi-public.
- No incentives are available for private fleet charging
 - In the first round, incentives were available for private charging; however, under this round incentives are limited to only public charging.

5/21 Updated NOFA

- **On May 21, 2026, the Board adopted an updated NOFA that extended the deadline from 5/31/2026 to 7/31/2026.**
- Provided clarifications based on Frequently Asked Questions.
- Link to [Updated NOFA](#).

FAQ: Eligibility

- Eligible applicants are businesses in Overburdened Communities (“OBC”)
- For an Applicant to “primarily operate” within a designated area, the fleet must operate in that identified territory for at least fifty-one percent (51%) of the vehicle miles traveled over the course of the compliance period.
- Businesses without fleets may also apply provided they subscribe multiple businesses to use the chargers.
- Nonprofits and government entities are not eligible for this grant but may be eligible for the [Clean Fleet](#) incentive.

Public & Quasi-Public Definitions

- “Publicly Available” refers to a charger or charger bank that is available and accessible, but not limited, to MHD vehicles that are useable by the public on a drive-up basis. Such depots must be accessible twenty-four (24) hours per day, seven (7) days per week and be listed on the United States Department of Energy Alternative Fueling Station Locator. “Accessible to MHD” includes, but is not limited to, utilizing pull-through stalls.
- “Quasi-Publicly Available” refers to a charger or charger bank that is available and accessible, but not limited, to MHD vehicles that the public can utilize on a subscription or scheduled basis. Such depots must be accessible twenty-four (24) hours per day, seven (7) days per week and be listed on the United States Department of Energy Alternative Fueling Station Locator. “Accessible to MHD” includes but is not limited to utilizing pull through stalls.

Eligible Equipment and Expenditures (1/2)

- DCFC Chargers
 - Dual-port
 - State compliant, networked DCFC [EVSE Compliant Network Service Providers](#)
- Eligible Expenses
 - Charger(s)
 - Taxes on the charger(s)
 - Warranty on the charger(s)
 - Delivery and activation fees for the charger(s)
 - Other expenses including software costs are **not** covered.

Eligible Equipment and Expenditures (2/2)

- Make Ready
- Eligible Expenses
 - The pre-wiring of electrical infrastructure
 - Includes service panels, junction boxes, conduit, wiring, etc., necessary to make a particular location able to accommodate EVSE on a “plug and play” basis.
 - Distribution system upgrades needed on site to accommodate the project

Application Process

Application Resource

<https://cleanenergy.nj.gov/mhd>

RGGI Medium and Heavy-Duty (MHD) EV Charging Grant Application Guide

DEADLINE:
July 31, 2026

Overview

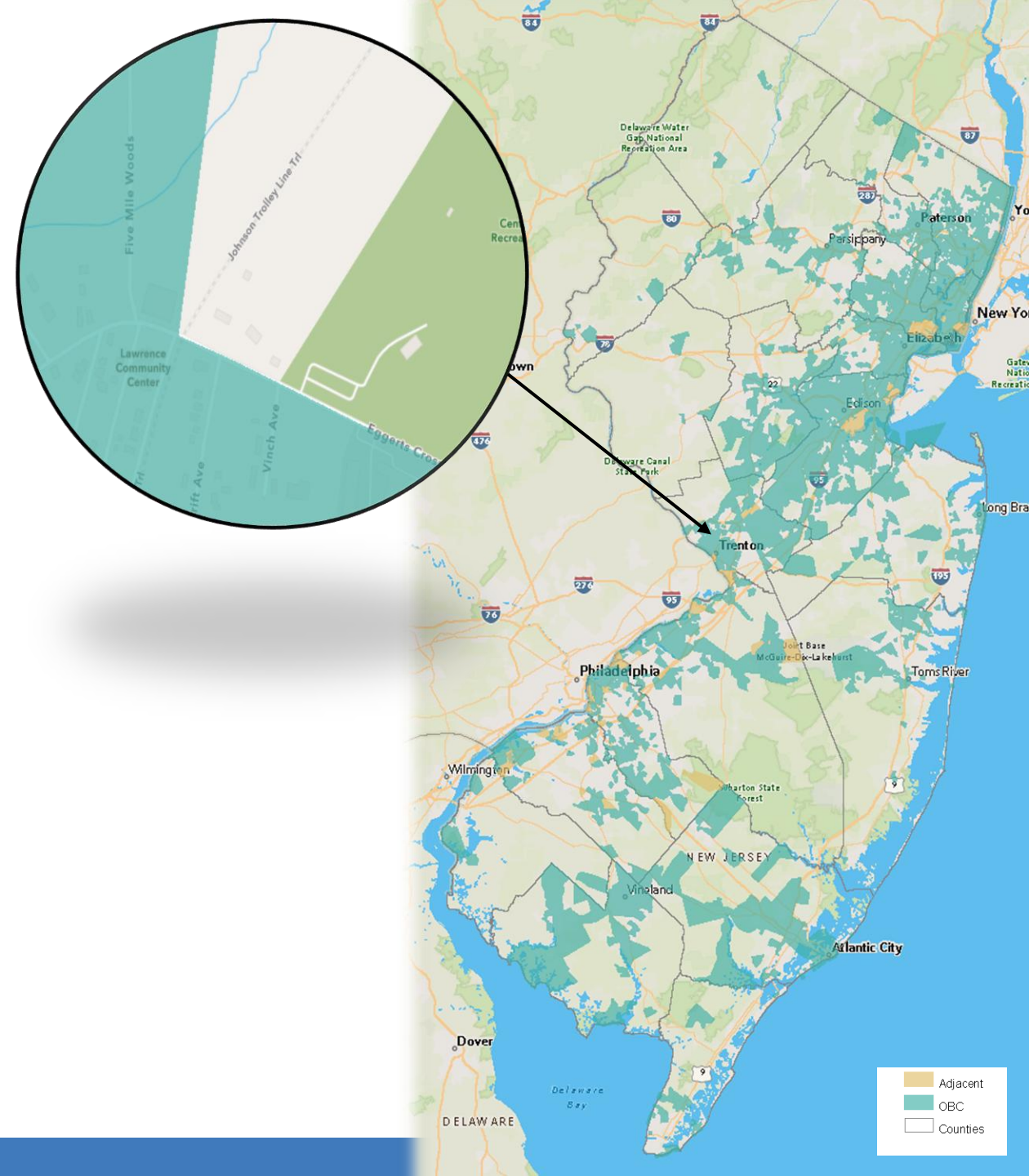
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New Jersey's
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program™

How do I know if my site is in an OBC?

- An interactive OBC map is available at <https://dep.nj.gov/ej/law/>
- PDFs are available at <https://dep.nj.gov/ej/communities/#municipalities> at the municipality level (scroll to the bottom and expand the applicable county)



Application Process (1/2)

1. Completing the application <https://njbpu.intelligrants.com/>
 - a. Select the MHD EV Charging Program application under the “My Opportunities” section of your home page in SAGE. Complete the application and save your progress. Do not submit till 1.b. is complete.
 - b. Concurrently, applicants must receive EDC review and upload documentation to SAGE.

Application Process (2/2)

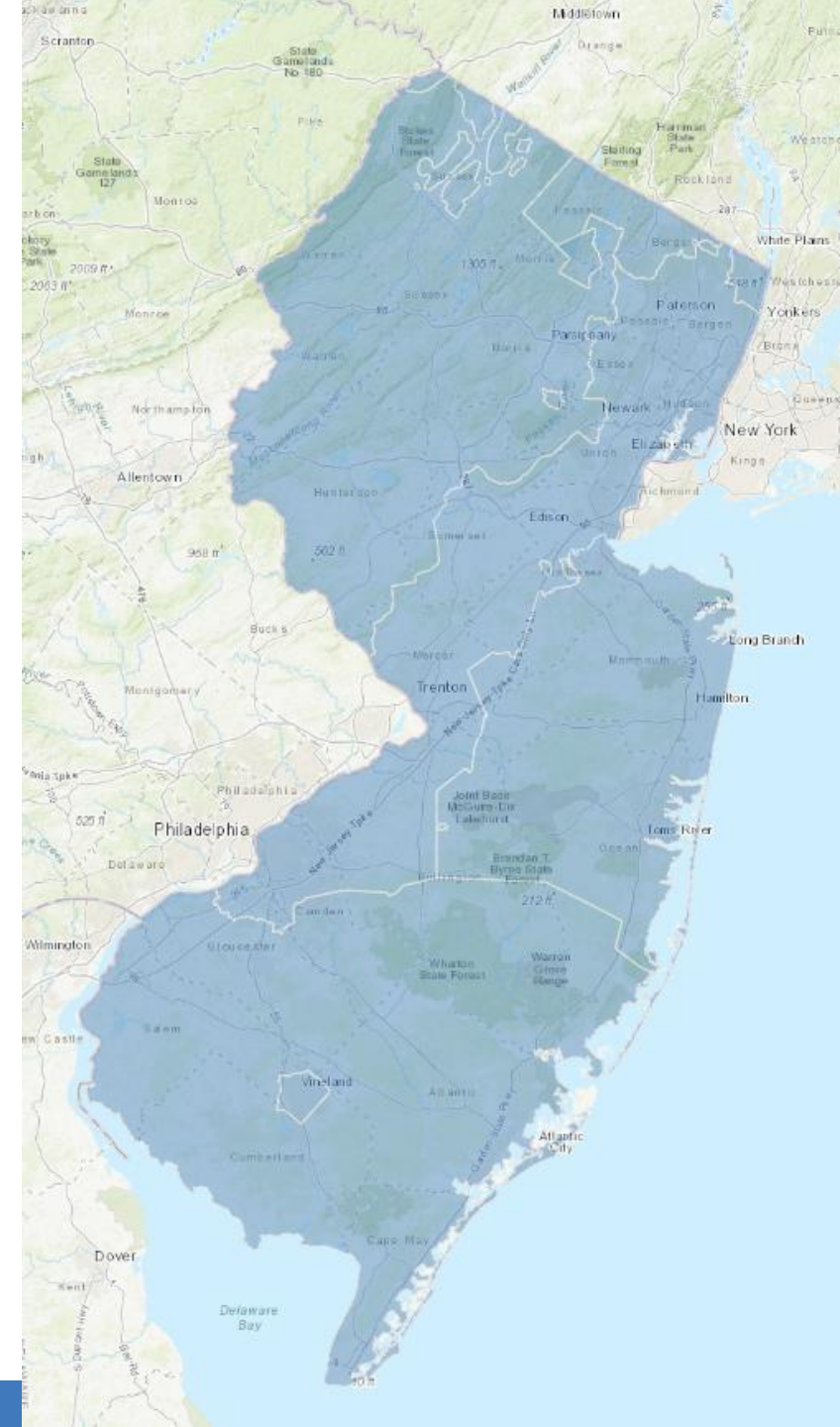
- 2. Submit the application by July 31, 2026 via SAGE.** Applications without EDC documentation will be deemed administratively incomplete and will be returned to the applicant for correction. All documentation from 1.a. and 1.b. must be submitted via SAGE by July 31, 2026. Only administratively complete applications submitted by the deadline will be considered. Extensions will not be granted.

EDC Review Overview

- **Talk to your utility early and often.**
- Applicants are required to confirm capacity through a utility capacity review.
 - Confirms site viability
 - Avoid unexpected expenses
- Applicants must submit documentation from their utility review in their application.

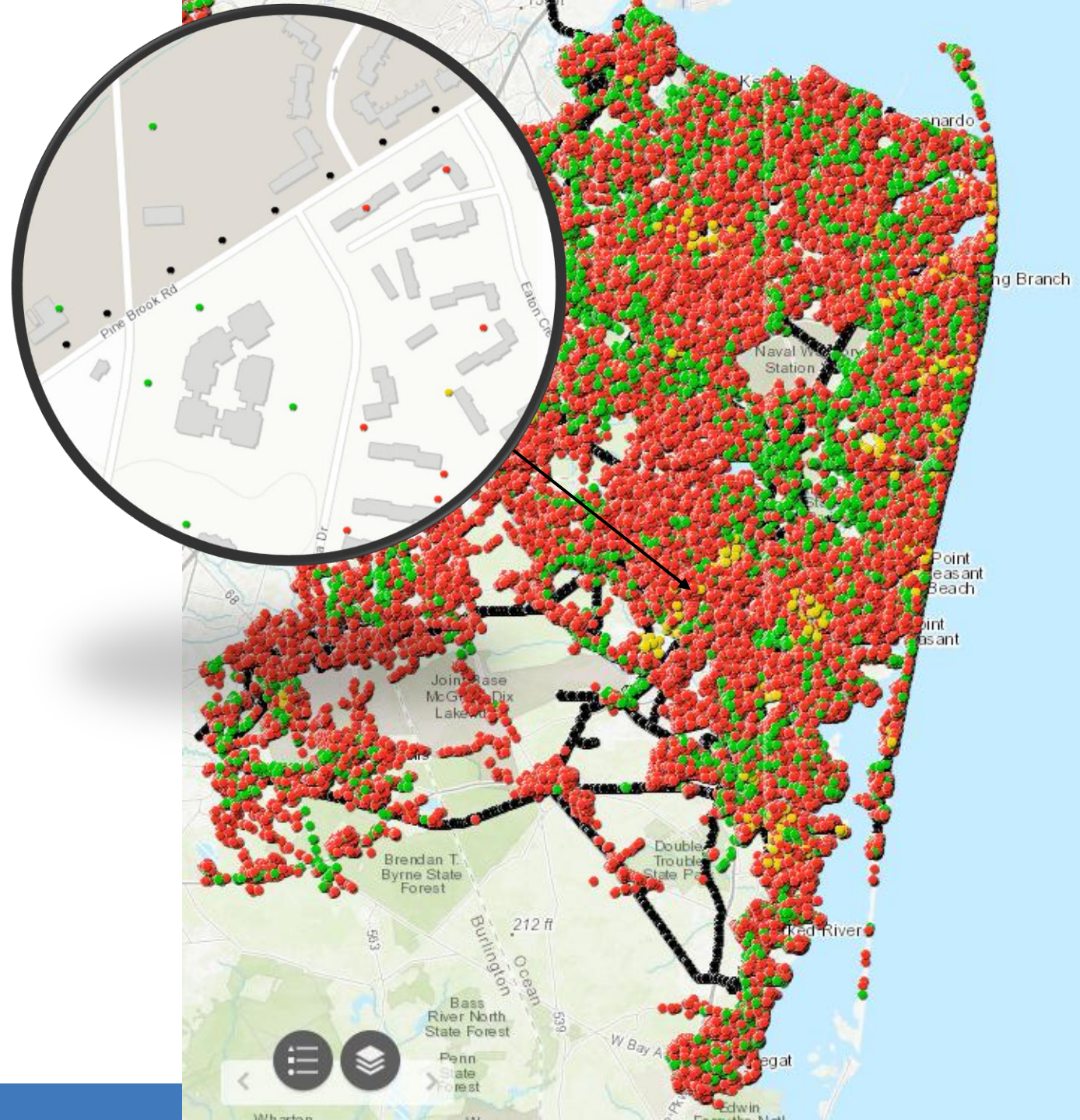
EDC Review Process

- To initiate request the review, Applicants must follow their utility's process.
- Utility service territories for any address can be located on <https://gisdata-njdep.opendata.arcgis.com/datasets/njdep::electric-utilities-territory-map-of-new-jersey/explore>.
- Detailed instructions can be found at: <https://njcleanenergy.com/EV>.



EDC Review Documentation (1/3)

1. A picture of a capacity map for the proposed site showing enough capacity for the proposed project's load. For some sites, more study is needed, and the EDCs cannot provide a capacity map.



EDC Review Documentation (2/3)

- The utility may determine that more study is needed.
- If the study determines there is capacity, the Applicant submit a letter from the EDC stating there is capacity.
- Lead times are approximately three (3) weeks, however, actual processing times vary and may increase during high volume EDC processing periods

EDC Review Documentation (3/3)

- If the EDC, during additional study discussed above, finds that there does not exist sufficient capacity for the proposed project, then the Applicant must request, at the Applicant's expense, a feasibility study to be performed by the EDC.
- After its findings are complete, Applicant must submit a letter from the EDC confirming the lack of capacity, the results of the EDC feasibility study, and a commitment that the Applicant will cover any additional costs and still intends to proceed with the project.

Timelines and Deadlines

Date	Activity
January 1, 2026 at 9:00 A.M. EST	The Program application will open and be available on SAGE.
January 6, 2026	Staff held a webinar providing an overview of the Program, application process, and EDC review.
January 29, 2026	Staff held a webinar providing an overview of the Program, application process, and EDC review.
April 16, 2026	Staff held a webinar providing an overview of the Program, application process, and EDC review. Staff also addressed frequent inquiries.
May 29, 2026 (Today)	Staff is holding a webinar to provide an overview of the Program, application process, and EDC review. Staff also is addressing frequent inquiries.
June, 2026	Staff will be holding a webinar to provide an overview of the Program, application process, and EDC review. Staff will also answer questions.
July, 2026	Staff will be holding a webinar to provide an overview of the Program, application process, and EDC review. Staff will also answer questions.
July 31, 2026 at 5:00 P.M. EST	The application window for the Program closes at 5:00 P.M. EST. Applications submitted after the deadline will not be accepted.

Resources





<https://cleanenergy.nj.gov/mhd>



Get Started ▾

Programs ▾

Resources ▾

About ▾

Support

Program

Electric Vehicles

Regional Greenhouse Gas Initiative (RGGI) Medium and Heavy Duty (MHD) EV Charging Program

The Regional Greenhouse Gas Initiative (RGGI) Medium- and Heavy-Duty (MHD) EV Charging Program supports the purchase of eligible direct current fast charger ("DCFC") EV equipment.



Application Resource

<https://cleanenergy.nj.gov/mhd>

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July 31, 2026

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Fleet

Electrification

Guidebook



[Link](#)



Public EV Charging Station Site Selection Checklist

The Joint Office of Energy and Transportation (Joint Office) provides technical assistance on planning and implementation of a national network of electric vehicle (EV) chargers and zero-emission fueling infrastructure, as well as zero-emission transit and school buses. There are several considerations that should be addressed when selecting a site for EV charging stations. The following is a checklist to assist with site selection for publicly available EV charging stations.

For more technical assistance resources please review [DriveElectric.gov/technical-assistance](https://driveelectric.gov/technical-assistance). If you would like detailed help or assistance with charging station site selection, please contact the Technical Assistance team at [DriveElectric.gov/contact](https://driveelectric.gov/contact).

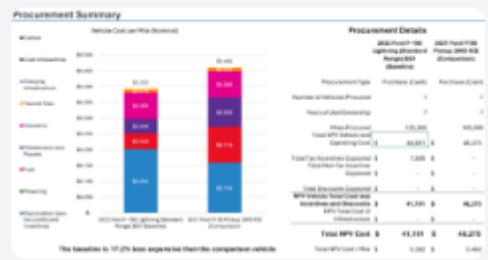
Fleet Procurement Analysis Tool

Last Feature Update: April 2026

By Nick Nigro

Past Support By [Daniel Wilkins](#), [James Di Filippo](#), [Josh Rosenberg](#), [Charles Satterfield](#)

 [Tools & Dashboards](#)



The Fleet Procurement Analysis Tool equips users with decision-relevant information on the financial viability and environmental impact of light-, medium-, and heavy-duty vehicle fleet procurements. The Microsoft Excel-based tool can evaluate a variety of procurement ownership structures, vehicle types, and procurement scenarios. The tool compares procurements side-by-side on a cost-per-mile basis and provides an analysis of cash flows and location-specific lifecycle emissions. The tool is highly flexible, supports customizable sensitivity variables, and produces user-friendly results summaries.

Other Funding

List of State and Utility Incentives

https://chargeup.njcleanenergy.com/ev-savings-hub

A ☆

[Available Funding](#) [FAQs](#) [Contact Us](#)



[Program Overview](#) [Car Shoppers](#) [Dealers](#) [Other Programs](#)

[Apply/Login](#)

New Jersey EV Savings Hub

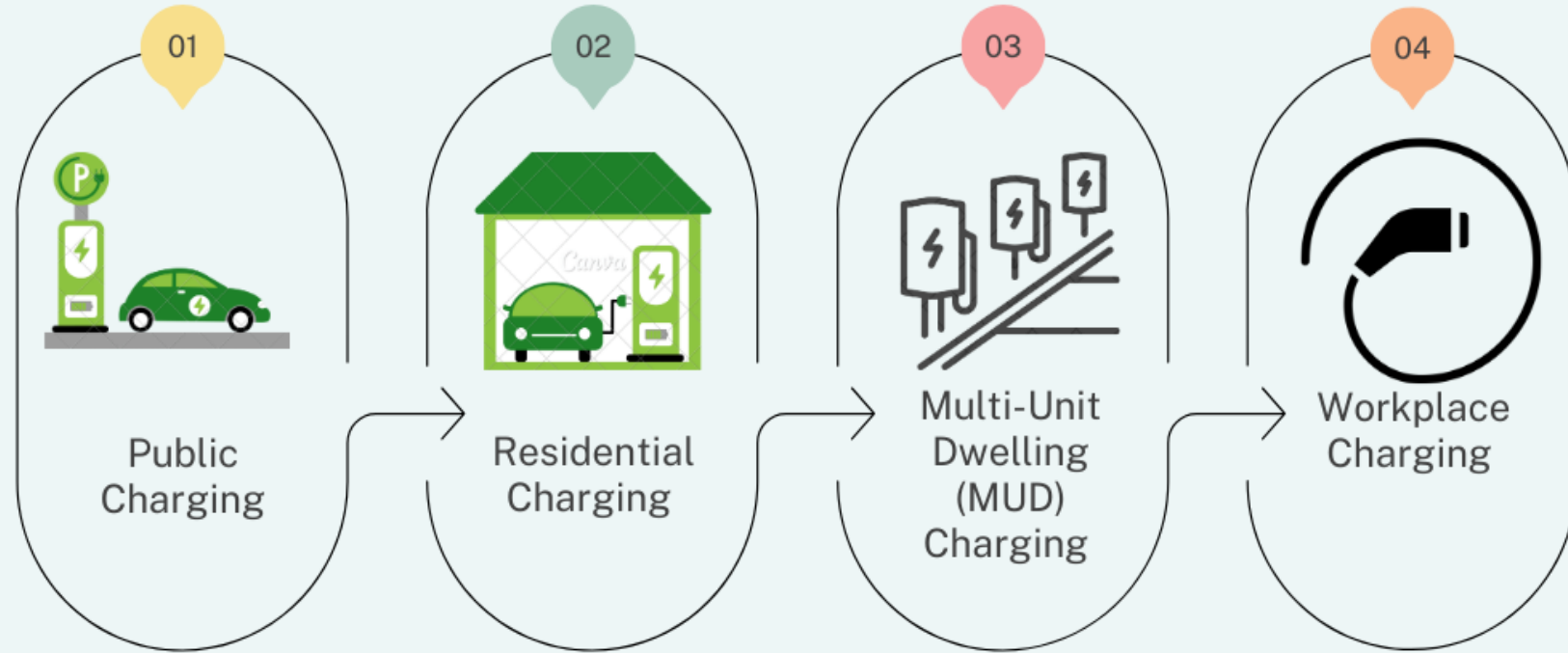


Other Funding

- BPU's MHD EV Charging Program **cannot** be stacked with other state funding, including but not limited to:
 - Department of Environmental Protection's It Pay\$ to Plug Program; and
 - Economic Development Authority's Charge Ahead Program
- BPU's MHD EV Charging Program **can** be stacked with upcoming utility incentives
 - EDCs currently have millions of dollars of funding for DCFC Make Ready
 - More money is coming through the MHD Proceedings.

Talk to your utility early and often!

Utility Incentives provide funding for the Make Ready for different EV charging use cases



PUBLICLY ACCESIBLE

*Available by schedule, subscription
or first-come, first-serve, open 24-7.
Designed for MHD vehicles (pull
through stalls, higher wattage)*

PUBLIC FLEETS

*Government owned fleets (ie
municipal, county, state, police,
school, NJ Transit)*

PUBLIC SERVING FLEETS

*Third-party providers of public
transportation (ie school buses,
providers of NJ Transit, etc)*

PRIVATE FLEETS

*Certain Private Fleets located in
or serving OBMs or OBCS along
Federal Freight EV Corridor*

TECHNICAL \$ PLANNING

*Available for all fleets, MUD, public
charging over 500 kW*

MHD Utility Filings Under Review

More Information

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